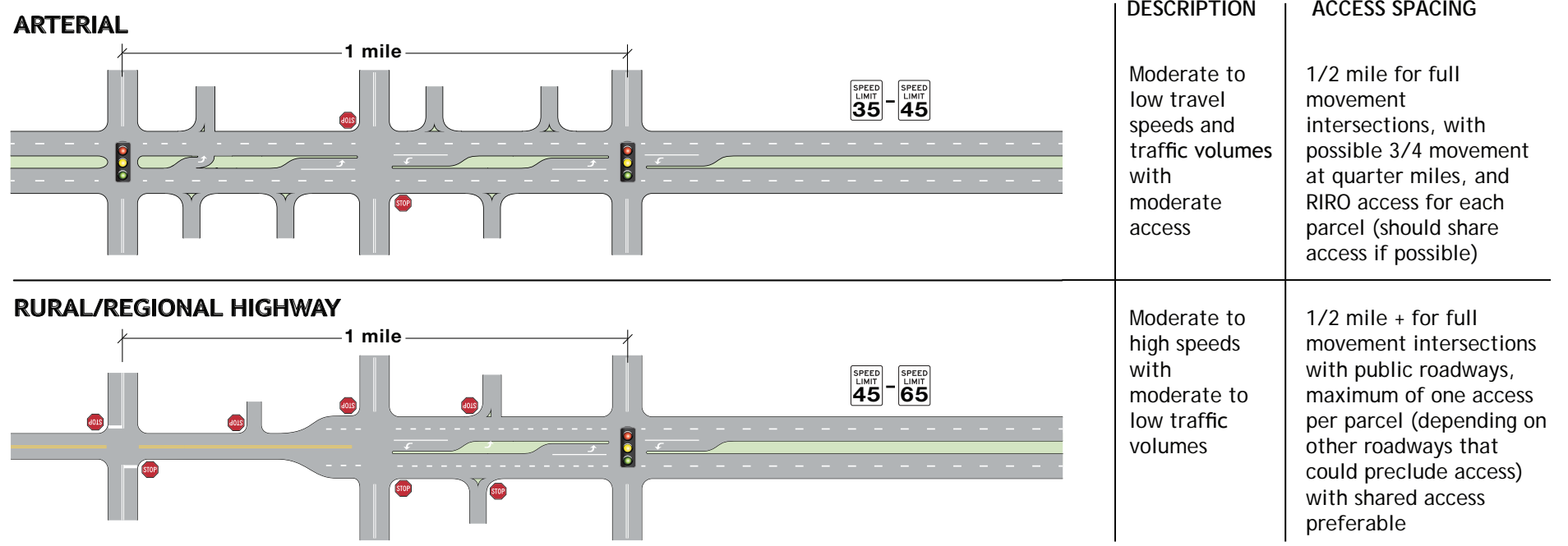


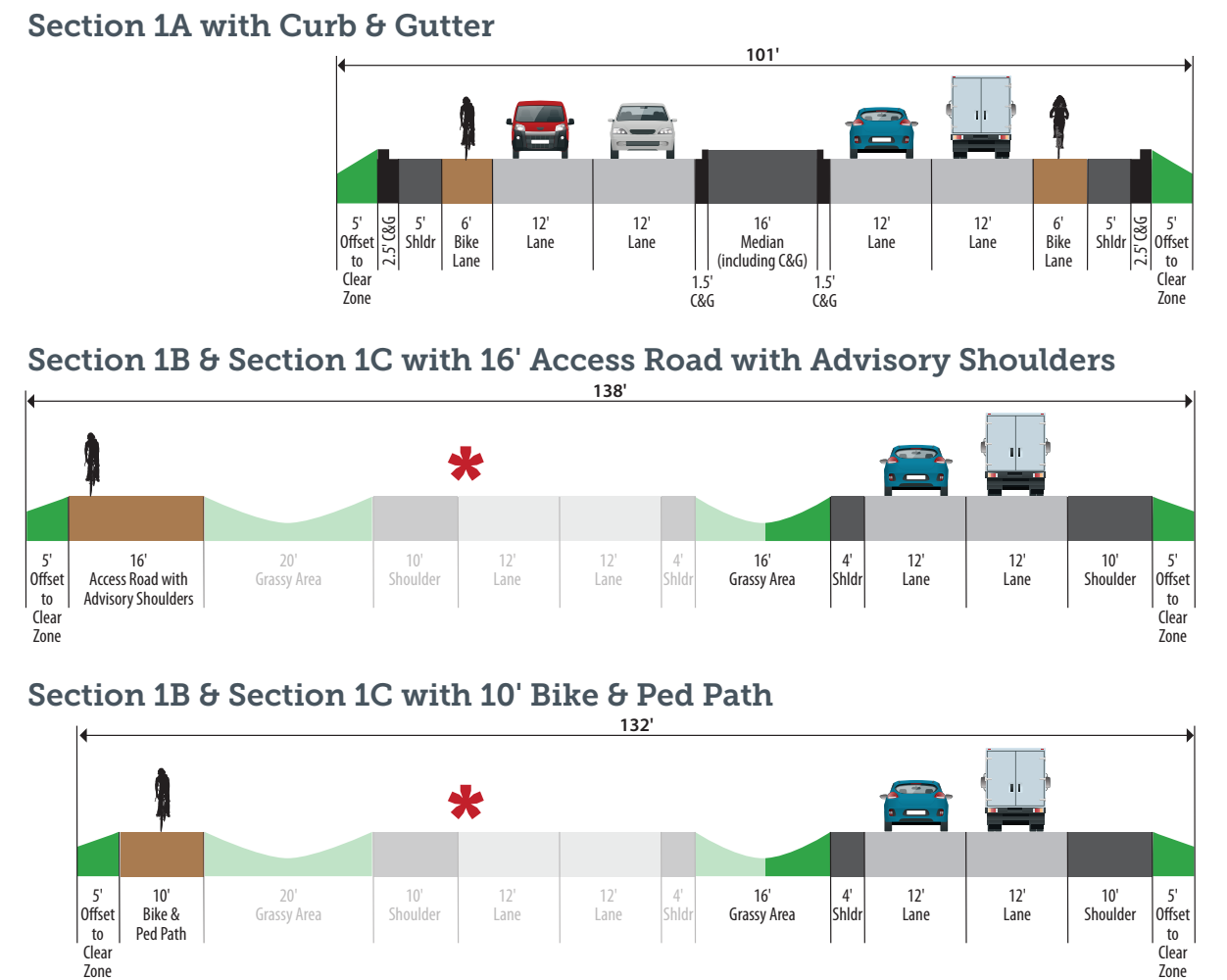
Overview & Recommendations

- Local agencies: Town of Lyons and Unincorporated Boulder County
- Known transportation problems: Vehicular access, mobility, and safety; bicycle and pedestrian connections and safety, bicycle crossings
- Existing roadway classification and lineage: Rural/Regional Highway with two to five lanes
- Recommended roadway classification:
 - Arterial roadway from McConnell Drive to Highland Drive (Section 1A)
 - Rural/Regional Highway from Highland Drive through 75th Street (Section 1B) and 75th Street through 87th Street (Section 1C)
- Total recommended cross section width: 101 feet to 138 feet
- Total right of way preservation acreage: 99.6 acres
- Recommended cross sections include:
 - Interim improvements include turn lanes at intersections
 - Four travel lanes (two 12-foot lanes in each direction) across all of Section 1 in the ultimate condition
 - A 16-foot wide median (raised with curb and gutter in Section 1A and grassy with cable barrier in Sections 1B and 1C)
 - Curb and gutter and bike lanes along SH 66 in Section 1A
 - Either a 10-foot bike and pedestrian path or a 16-foot access road with advisory shoulders along SH 66 in Section 1B and 1C
 - A five-foot offset to clear zone (a clear zone is an unobstructed, traversable roadside area that allows a driver to stop safely or regain control of a vehicle that has left the roadway) in areas that are not curb & gutter

Roadway Classification



Recommended Cross Sections (facing east)



* Right-of-way preservation for potential multi-modal and safety transportation improvements

Local Agency Planning Efforts

- Lyons vision for:**
- Business district along SH 66
 - US 36/SH 66 roundabout
 - Gateway features at US 36/SH 66 and east of US 36 along SH 66

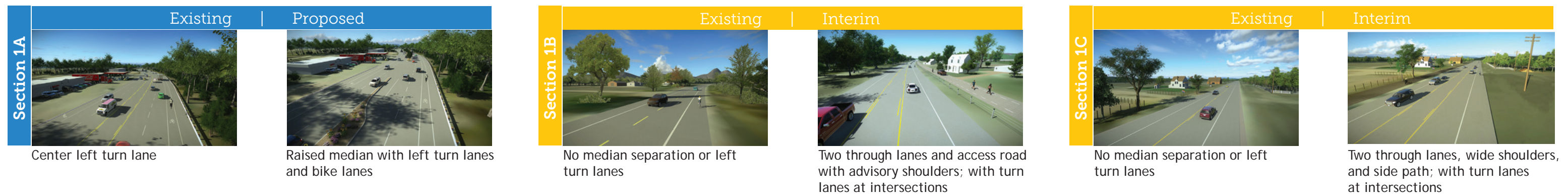
- Boulder County vision for:**
- Improve bus service and stops, park and ride capacity, and local transit connections; add queue jump lanes
 - Incorporate bikeable shoulders and key grade separated crossings
 - Enhance intersections to improve safety and convenience for all modes and to reduce congestion

For more information, please view the SH 66 PEL Corridor Conditions Report (Appendix C).

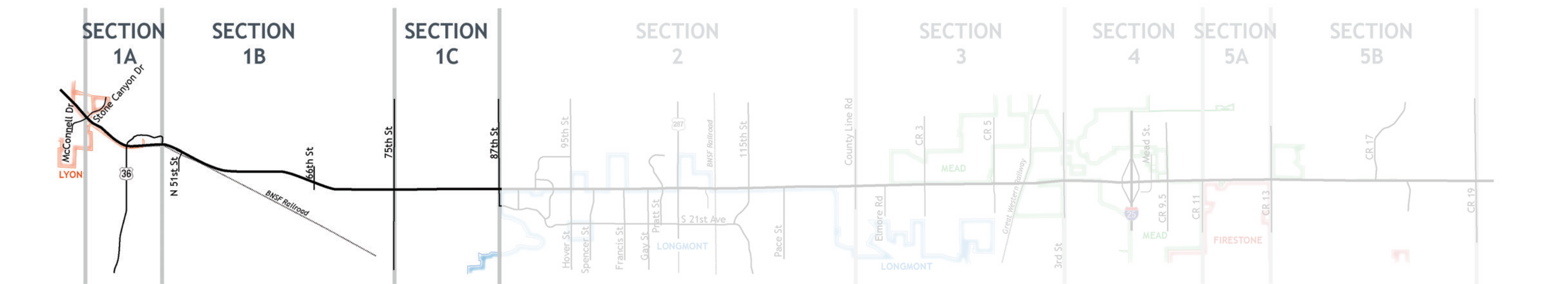
Recommended Right-of-Way Preservation Footprint



Existing & Proposed Visualizations (facing west)



Improvements



Near-Term 0-10 years	LOCATION ON SH 66		RECOMMENDED IMPROVEMENT
	SECTION	(intersection or section)	
	1A	McConnell Drive to Highland Drive East	Install raised median and restrict and/or consolidate accesses. Install rumble strips and/or bike lanes.
	1A	US 36	Construct grade-separated underpass for bicycle and pedestrians.
	1B	Section-wide	Install rumble strips.
	1B	Section-wide	Install access road with advisory shoulders, add right and left turn lanes at those accesses; and install sidepath.
	1B	75th Street	Re-assess signal timing. Install bicycle and pedestrian grade-separated crossing.
	1C	Section-wide	Install rumble strips. Install access road with advisory shoulders, add right and left turn lanes at those accesses; and install sidepath.

Long-Term 10-20 years	LOCATION ON SH 66		RECOMMENDED IMPROVEMENT
	SECTION	(intersection or section)	
	1A	Section-wide	Work with local agencies to install trail along SH 66.
	1B	53rd Street	Install bicycle and pedestrian grade-separated crossing.

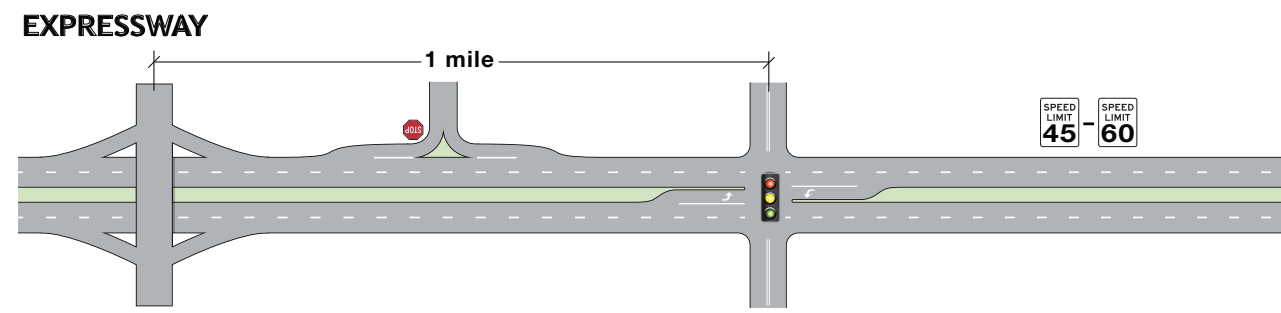
Beyond Horizon Year +20 years	LOCATION ON SH 66		RECOMMENDED IMPROVEMENT
	SECTION	(intersection or section)	
	1B	Section-wide	Increase capacity of highway from 2 to 4 lanes.
	1C	Section-wide	Increase capacity of highway from 2 to 4 lanes.

Mid-Term 5-15 years	LOCATION ON SH 66		RECOMMENDED IMPROVEMENT
	SECTION	(intersection or section)	
	1B	Section-wide	Construct missing Sections of access road with advisory shoulders, and/or bike/ped only connections. Include shoulder widening. Work with local agencies to construct trail along BNSF.
	1C	Section-wide	Construct missing Sections of access road with advisory shoulders, and/or bike/ped only connections. Include shoulder widening.

Overview & Recommendations

- Local agencies: City of Longmont and Unincorporated Boulder County
 - Known transportation problems: Vehicular access, mobility, and safety; bicycle and pedestrian mobility and safety; transit access
 - Existing roadway classification and lineage: Non-rural principal highway with two to four lanes
 - Recommended roadway classification: Expressway
 - Total recommended cross section width: 109 feet to 145 feet
 - Total right of way preservation acreage: 82.8 acres
- Recommended cross sections include:
 - Four travel lanes (two 12-foot lanes in each direction)
 - A 16-foot wide median (raised with curb and gutter)
 - Five-foot outside shoulders
 - A 10-foot bike and pedestrian path on one or both sides of the road
 - A 16-foot frontage road along SH 66 in select locations
 - A five-foot offset to clear zone (a clear zone is an unobstructed, traversable roadside area that allows a driver to stop safely or regain control of a vehicle that has left the roadway)
 - Addition of turn lanes at intersections

Roadway Classification



DESCRIPTION	ACCESS SPACING
Moderately high speeds and traffic volumes with limited access, multiple lanes in each direction and separated directional travel	1 mile + for full movement, with possible RIRO at half mile

Local Agency Planning Efforts



Longmont vision for:

- Side path from 87th Street to County Line Road
- Two underpasses at SH 66/US 287 and SH 66/Pace Street
- Tie bike lanes into north-south routes along SH 66
- SH 66/US 287 Park-n-Ride
- Active participation with CDOT for multi-modal plan



Boulder County vision for:

- Improve bus service and stops, park and ride capacity, and local transit connections; add queue jump lanes
- Incorporate bikeable shoulders and key grade separated crossings
- Enhance intersections to improve safety and convenience for all modes and to reduce congestion



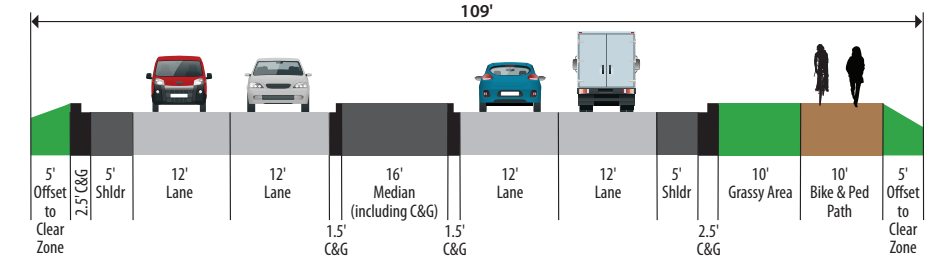
For more information, please view the SH 66 PEL Corridor Conditions Report (Appendix C).

Recommended Right-of-Way Preservation Footprint

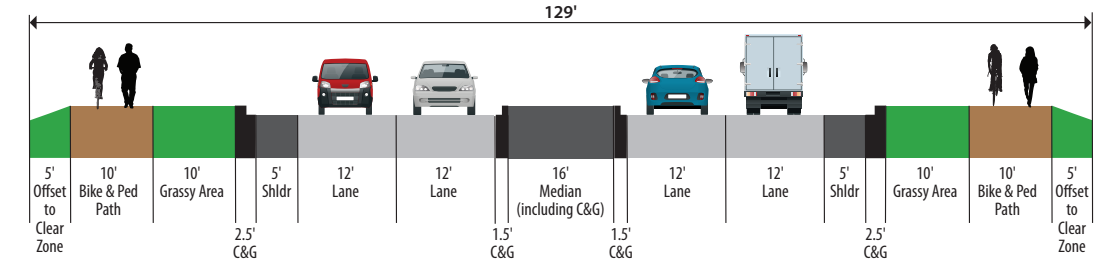


Recommended Cross Sections (facing east)

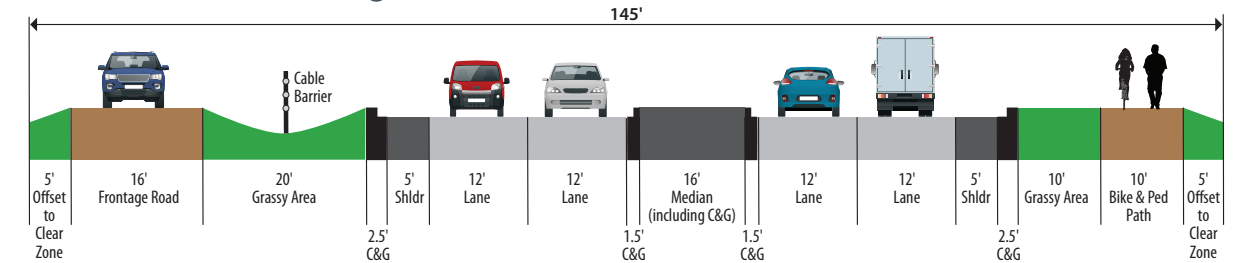
Section 2 with Curb & Gutter and 10' Bike & Ped Path



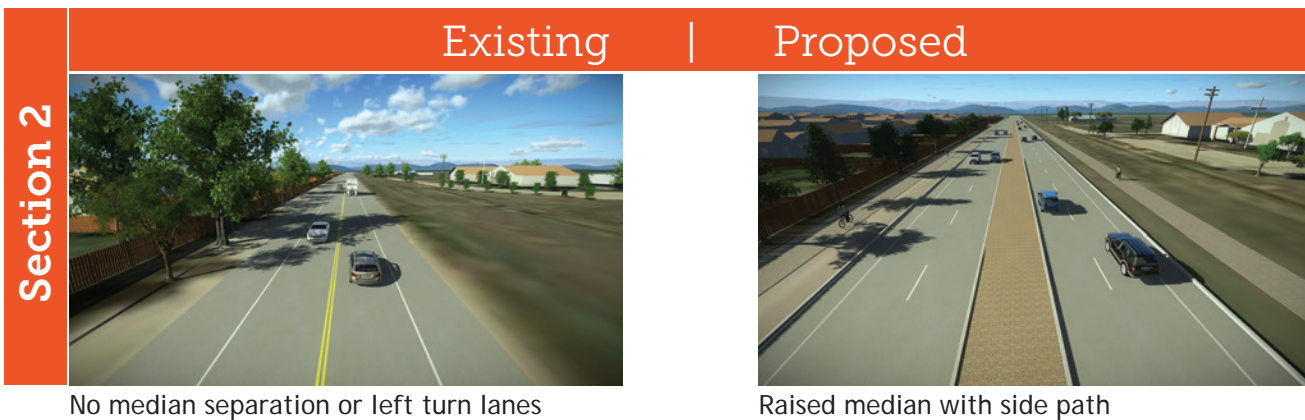
Section 2 with Curb & Gutter and 10' Bike & Ped Path on North & South



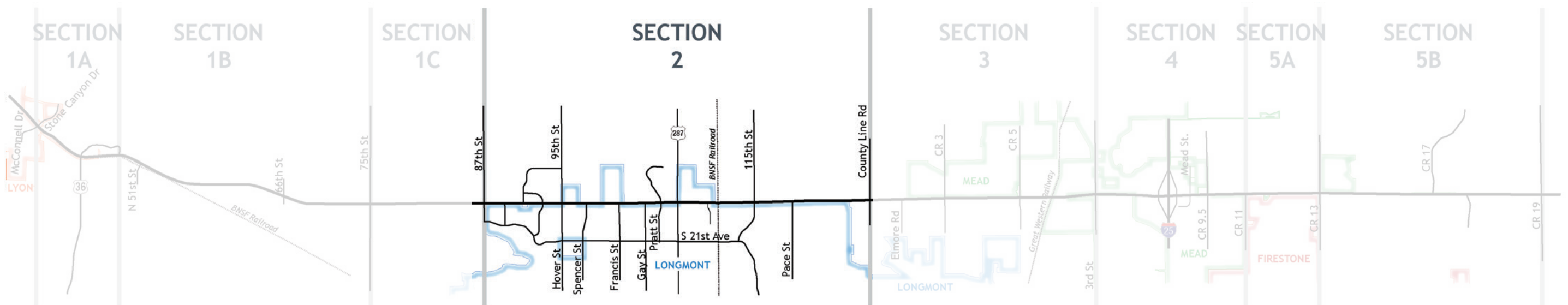
Section 2 with Frontage Road



Existing & Proposed Visualizations (facing west)



Improvements



Near-Term 0-10 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	SECTION		
	2	Section-wide	Restrict left turns out where identified in the ACP. Install sidepath on the south side of SH 66. Monitor and/or perform a warrant analysis at intersections where signals are planned in the ACP (Airport Road, Francis St, Alpine St, Sundance Dr).
	2	Hover St/95th Street	Add auxiliary lanes and capacity.
	2	SH 66 from Hover St to US 287	Expand SH 66 to four lanes plus auxiliary lanes between Hover/95th St and US 287. Install median and implement access control where there are high delays and/or high crash rates on side streets.
	2	US 287	Improve intersection: recommend carrying forward displaced left turn or other alternative intersection option.
	2	Alpine Street to County Line Rd	Improve SH 66 from 2 to 4 lanes, add a median, and install auxiliary lanes at intersections between Alpine St and County Line Road.

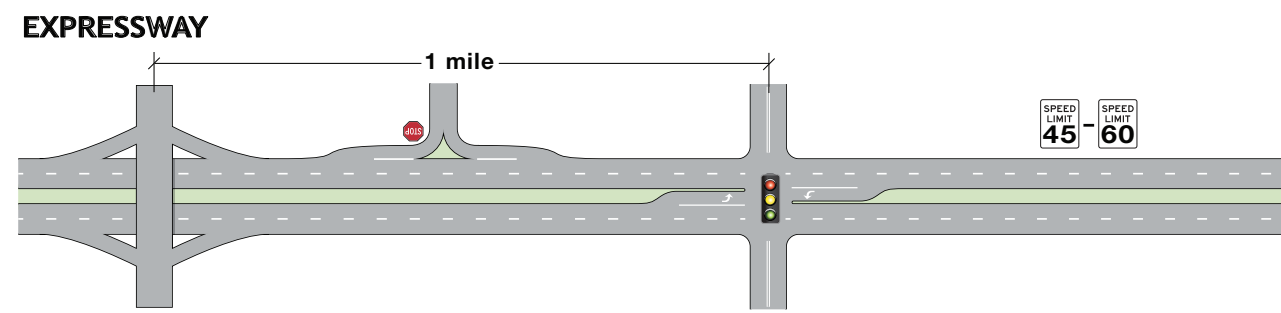
Mid-Term 5-15 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	SECTION		
	2	Section-wide	Monitor and restrict left turns out where identified in the ACP and a documented safety need is shown. Include shoulder widening. Install sidepath on the north side of SH 66. Monitor and/or perform a warrant analysis at intersections where signals are identified as allowed in the ACP (Airport Road, Francis St, Alpine St, Sundance Dr).
	2	Erfert St to Alpine St	Grade-separate SH 66 over railroad, improve the highway from 2 to 4 lanes, add a median, and install turn lanes where warranted at intersections.

Long-Term 10-20 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT	
	SECTION			
		2	Section-wide	Monitor and restrict left turns out where identified in the ACP and a documented safety need is shown. Monitor and/or perform a warrant analysis at intersections where signals are identified as allowed in the ACP (Airport Road, Francis St, Alpine St, Sundance Dr).
		2	87th Street to 95th Street	Improve the highway from 2 to 4 lanes, add a median, and install turn lanes where warranted at intersections.
	2	BNSF and Pace	Install bicycle and pedestrian grade-separated crossing lanes where warranted at intersections.	
	2	BNSF and Pace	Install bicycle and pedestrian grade-separated crossing.	

Overview & Recommendations

- Local agencies: City of Longmont, Town of Mead, and Unincorporated Weld County
- Known transportation problems: Vehicular access, mobility, and safety; bicycle connections and safety
- Existing roadway classification and lineage: Rural/regional highway with two lanes
- Recommended roadway classification: Expressway
- Total recommended cross section width: 122 feet
- Total right of way preservation acreage: 45.3 acres
- Recommended cross section includes:
 - Four travel lanes (two 12-foot lanes in each direction)
 - A 16-foot wide grassy median with cable barrier
 - A 10-foot bike and pedestrian path along SH 66
 - 10-foot shoulders
 - A five-foot offset to clear zone (a clear zone is an unobstructed, traversable roadside area that allows a driver to stop safely or regain control of a vehicle that has left the roadway)
 - Addition of turn lanes at intersections

Roadway Classification



DESCRIPTION	ACCESS SPACING
Moderately high speeds and traffic volumes with limited access, multiple lanes in each direction and separated directional travel	1 mile + for full movement, with possible RIRO at half mile

Local Agency Planning Efforts

- Longmont vision for:**
- Side path from 87th Street to County Line Road
 - Two underpasses at SH 66/US 287 and SH 66/Pace Street
 - Tie bike lanes into north-south routes along SH 66
 - SH 66/US 287 Park-n-Ride
 - Active participation with CDOT for multi-modal plan



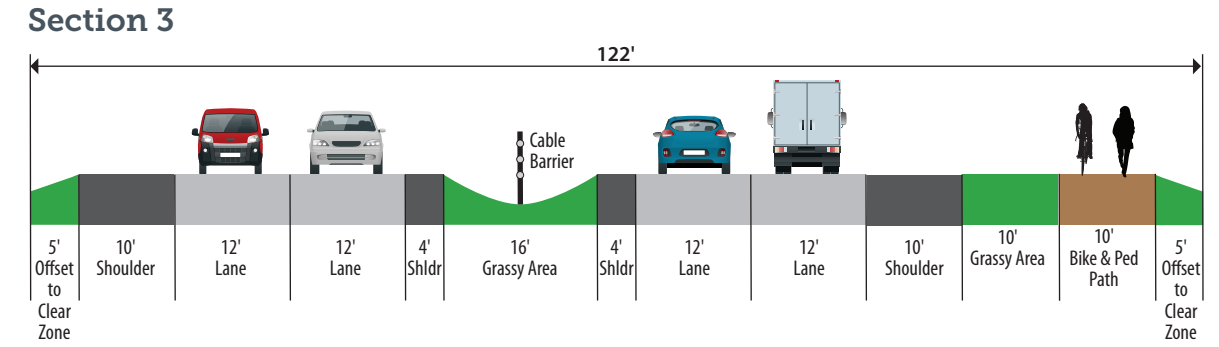
- Mead vision for:**
- Gateways at I-25/SH 66, SH 66/WCR 1, and SH 66/WCR 7 (3rd Street)
 - SH 66/3rd Street intersection improvements and signalization
 - Widen SH 66
 - Proposed trail



- Weld County vision for:**
- Trail connections
 - Access control
 - Intersection improvement partnership SH 66/WCR 7 (3rd Street)

For more information, please view the SH 66 PEL Corridor Conditions Report (Appendix C).

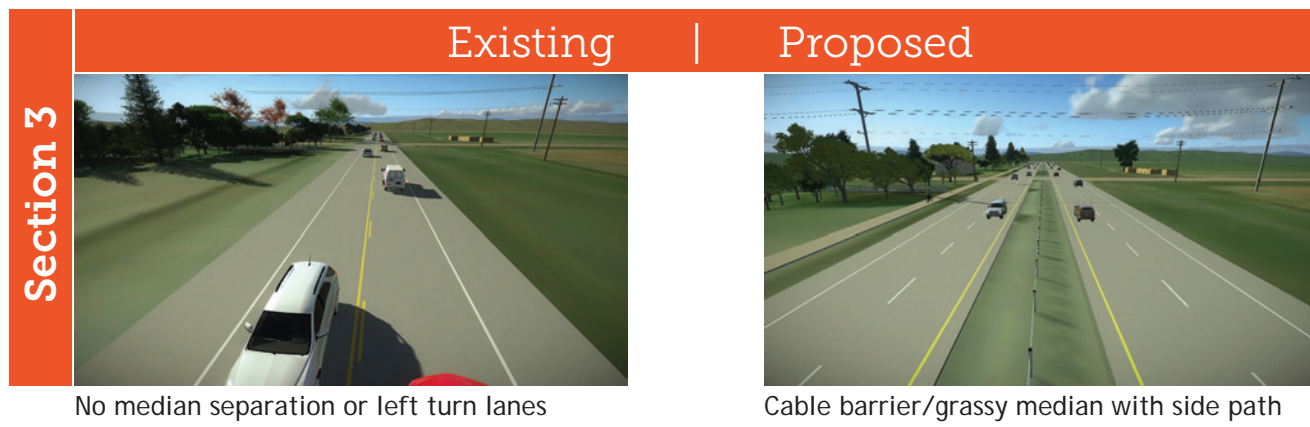
Recommended Cross Section (facing east)



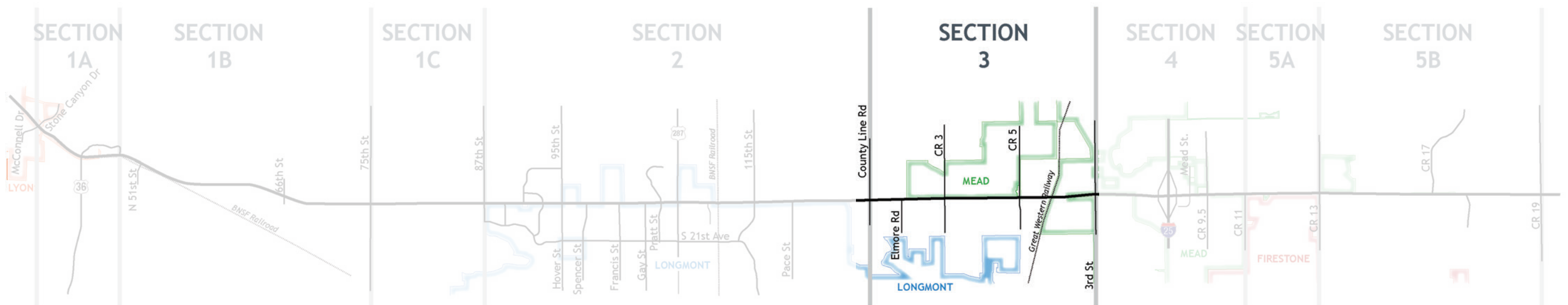
Recommended Right-of-Way Preservation Footprint



Existing & Proposed Visualizations (facing west)



Improvements



Near-Term 0-10 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	SECTION		
	3	Section-wide	Install rumble strips along corridor; add auxiliary lanes where warranted at intersections.
	3	Section-wide	Restrict left turns out where identified in the ACP and a documented safety need is shown. Monitor and/or perform a warrant analysis where signals are planned in the ACP (WCR 3, WCR 5, WCR 5.5 (interim condition only)).

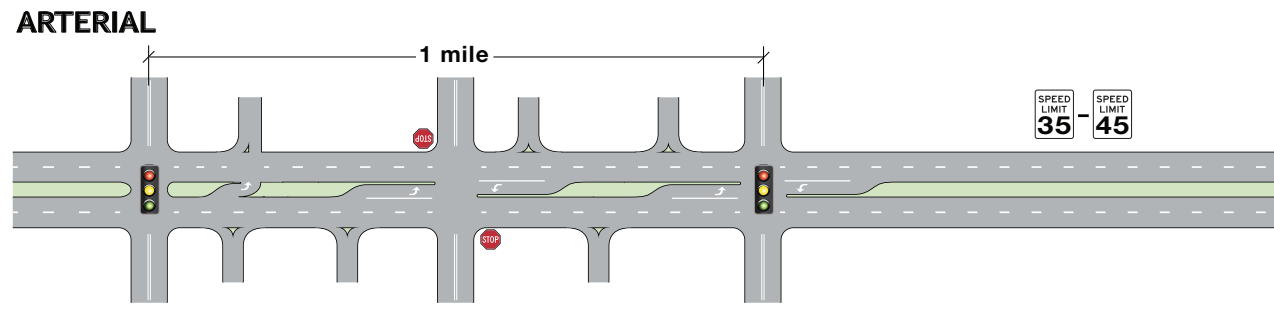
Long-Term 10-20 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	SECTION		
	3	WCR 5	Install bicycle and pedestrian grade-separated crossing east of WCR 5.
	3	Section-wide	Monitor and restrict left turns out where identified in the ACP and a documented safety need is shown. Monitor and/or perform a warrant analysis where signals are identified as allowed in the ACP (WCR 3, WCR 5, WCR 5.5 (interim condition only)).

Mid-Term 5-15 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	SECTION		
	3	Section-wide	Monitor and restrict left turns out where identified in the ACP and a documented safety need is shown. Include shoulder widening. Install sidepath. Monitor and/or perform a warrant analysis where signals are identified as allowed in the ACP (WCR 3, WCR 5, WCR 5.5 (interim condition only)).
	3	County Line Road to WCR 7	Improve the highway from 2 to 4 lanes, add a median, and install turn lanes where warranted at intersections.

Overview & Recommendations

- Local agencies: Town of Mead and Unincorporated Weld County
 - Known transportation problems: Access, mobility, and safety concerns for vehicles, bicycles, pedestrians, and transit
 - Existing roadway classification and lineage: Non-rural Principal Highway with two to three lanes east of I-25; four to five lanes west of I-25
 - Recommended roadway classification: Arterial roadway
 - Total recommended cross section width: 133 feet to 143 feet
 - Total right of way preservation acreage: 45.2 acres
- Recommended cross sections include:
 - Six travel lanes (three 12-foot lanes in each direction) and five-foot outside shoulders
 - A raised 16-foot wide median
 - Curb and gutter along SH 66
 - A 10-foot bike and pedestrian path along SH 66 setback 5 feet from SH 66 on both sides of SH 66 at select locations
 - A 5-foot offset to clear zone (a clear zone is an unobstructed, traversable roadside area that allows a driver to stop safely or regain control of a vehicle that has left the roadway)
 - Addition of turn lanes at intersections

Roadway Classification



DESCRIPTION	ACCESS SPACING
Moderate to low travel speeds and traffic volumes with moderate access	1/2 mile for full movement intersections, with possible 3/4 movement at quarter miles, and RIRO access for each parcel (should share access if possible)

Local Agency Planning Efforts



Mead vision for:

- Gateways at I-25/SH 66, and SH 66/WCR 7 (3rd Street)
- SH 66/3rd Street intersection improvements and signalization
- Widen SH 66
- Proposed trail connections



Weld County vision for:

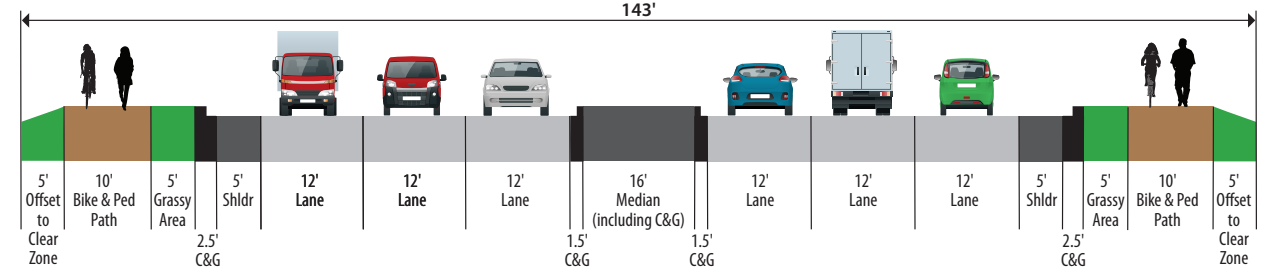
- Trail connections
- Access control
- Intersection improvement partnership at SH 66/WCR 7 (3rd Street)



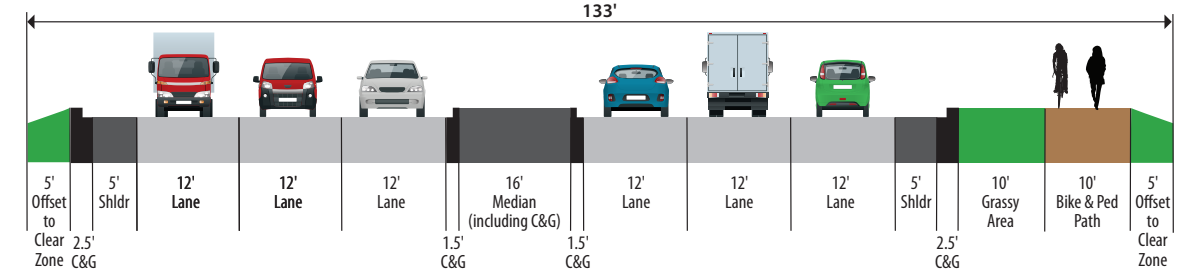
For more information, please view the SH 66 PEL Corridor Conditions Report (Appendix C).

Recommended Cross Sections (facing east)

Section 4 with Curb & Gutter and Bike & Ped Path on North & South



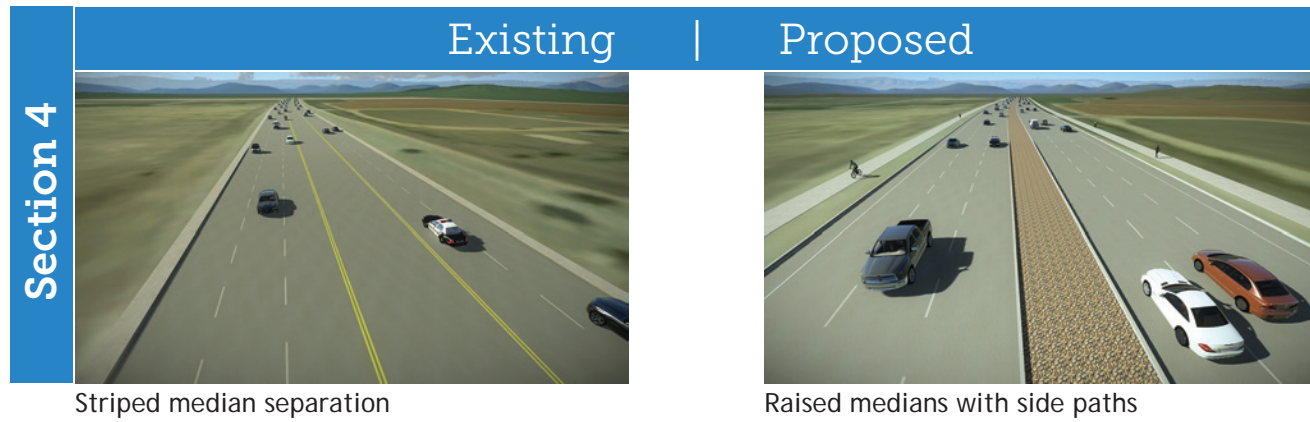
Section 4 with Curb & Gutter and Bike & Ped Path



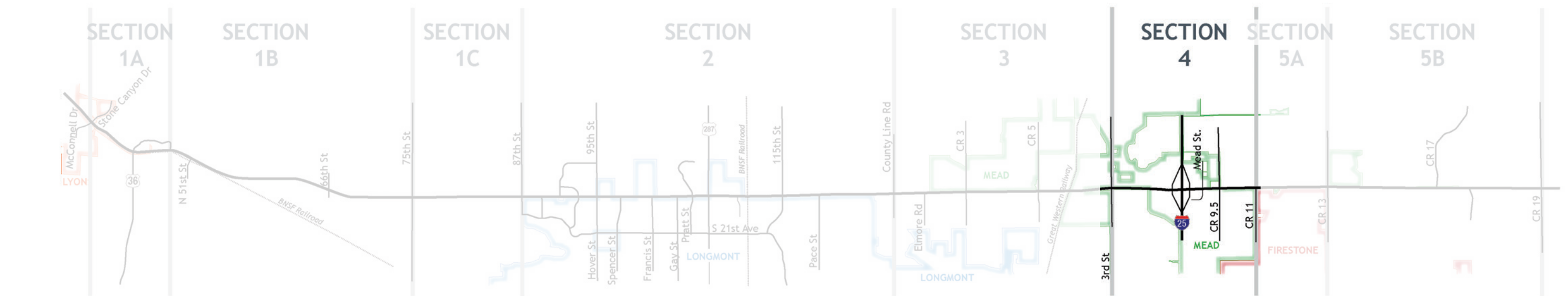
Recommended Right-of-Way Preservation Footprint



Existing & Proposed Visualizations (facing west)



Improvements



Near-Term 0-10 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	SECTION		
4	Section-wide		Install sidepath on north side of SH 66.
4	WCR 7		Add auxiliary lanes and capacity on SH 66 and/or CR 7 (design currently underway by CDOT/nearby developers).
4	Mead Street		Restrict left turns out if a documented safety need is shown.

Long-Term 10-20 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	SECTION		
4	Section-wide		Improve the highway from 4 to 6 lanes between WCR 7 and WCR 9.5. Install sidepath on south side of SH 66. Install bicycle and pedestrian grade-separate crossing east of WCR 7.
4	WCR 9.5		Grade-separate intersection; recommend carrying forward split intersection/partial interchange or other alternative intersection option.

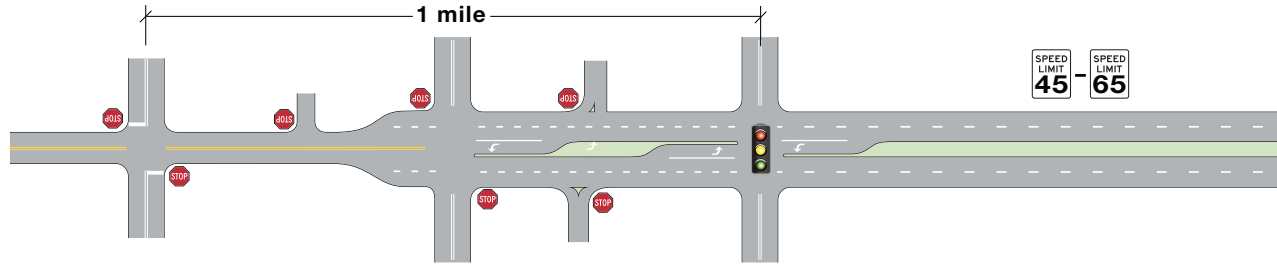
Mid-Term 5-15 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT		
	SECTION				
	4	Section-wide			Include shoulder widening.
	4	Gap Section between WCR 7 and Foster Ridge Dr			Improve the highway from 2 to 4 lanes, add a median, and install turn lanes where warranted at intersections. Signalize Foster Ridge Dr when warranted.
	4	WCR 9.5 to WCR 11			Improve the highway from 2 to 4 lanes, add a median, and install turn lanes where warranted at intersections.
4	Mead Street		Monitor intersection operations and restrict left turns out if a documented safety need is shown.		
4	WCR 9.5		Add turn lanes and capacity to intersection		

Overview & Recommendations

- Local agencies: Town of Mead and Unincorporated Weld County
- Known transportation problems: Vehicular access, mobility, and safety; bicycle and pedestrian mobility and safety
- Existing roadway classification and lineage: Non-rural Principal Highway and Rural/Regional Highway with two lanes
- Recommended roadway classification: Rural/Regional Highway
- Total recommended cross section width: 122 feet
- Total right of way preservation acreage: 67.9 acres
- Recommended cross section includes:
 - Four travel lanes (two 12-foot lanes in each direction)
 - A 16-foot wide grassy median with cable barrier
 - A 10-foot bike and pedestrian path along SH 66
 - 10-foot shoulders
 - A five-foot offset to clear zone (a clear zone is an unobstructed, traversable roadside area that allows a driver to stop safely or regain control of a vehicle that has left the roadway)
 - Addition of turn lanes at intersections

Roadway Classification

RURAL/REGIONAL HIGHWAY



DESCRIPTION
Moderate to high speeds with moderate to low traffic volumes

ACCESS SPACING
1/2 mile + for full movement intersections with public roadways, maximum of one access per parcel (depending on other roadways that could preclude access) with shared access preferable

Local Agency Planning Efforts

Mead vision for:

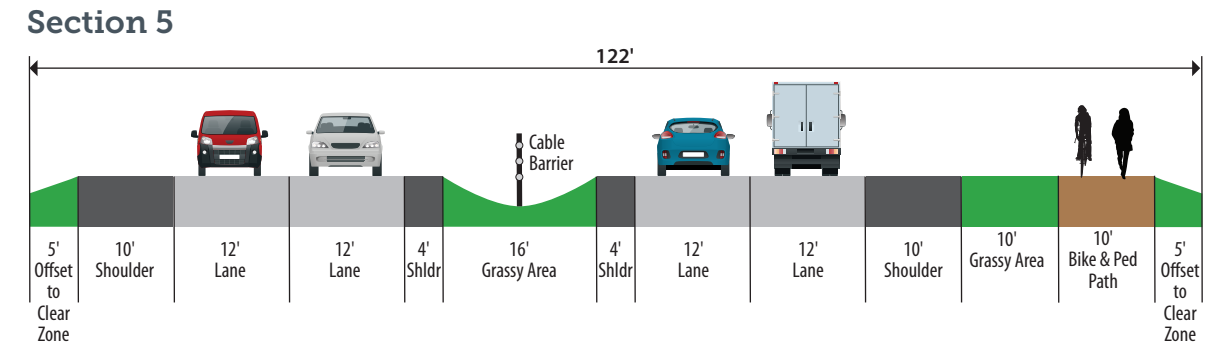
- Widen SH 66
- Proposed trail connections

Weld County vision for:

- Trail connections
- Access control
- Intersection improvement partnerships at SH 66/ WCR 13

For more information, please view the SH 66 PEL Corridor Conditions Report (Appendix C).

Recommended Cross Section (facing east)



Recommended Right-of-Way Preservation Footprint



Existing & Proposed Visualizations (facing west)

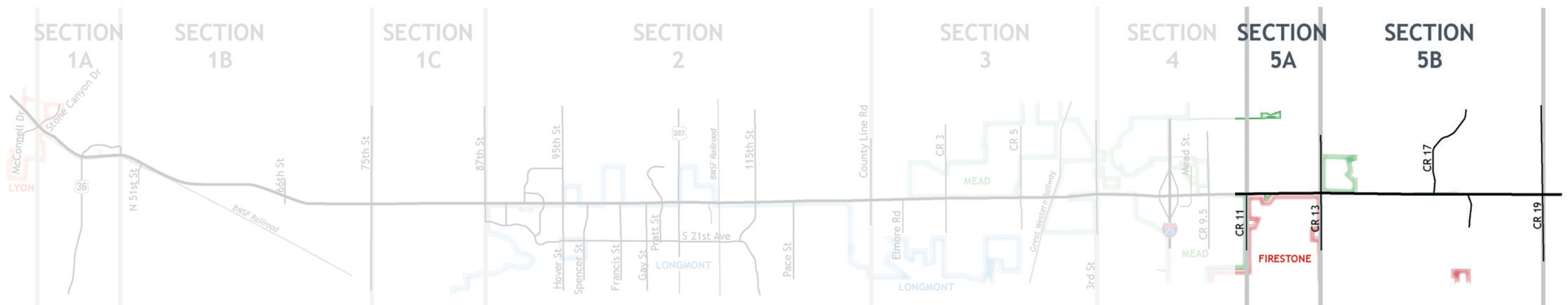
Section 5A

Existing	Proposed
No median separation or left turn lanes	Cable barrier/grassy median with side path

Section 5B

Existing	Proposed
No median separation or left turn lanes	Cable barrier/grassy median with side path

Improvements



Near-Term 0-10 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	5A	Section-wide	Monitor and/or perform a warrant analysis at intersections where signals are planned in the ACP (WCR 11, WCR 11.5 when constructed).
	5A	Section-wide	Install rumble strips and widen shoulders.
	5B	Section-wide	Install rumble strips and widen shoulders.
	5B	WCR 13	Install eastbound and westbound right turn lane.

Long-Term 10-20 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	5A	Section-wide	Monitor and/or perform a warrant analysis at intersections where signals are identified as allowed in the ACP (WCR 11, WCR 11.5 when constructed). Install sidepath on south side of SH 66.
	5B	Section-wide	Monitor and/or perform a warrant analysis at intersections where signals are identified as allowed in the ACP (WCR 11, WCR 19, WCR 11.5 when constructed). Install sidepath on south side of SH 66.

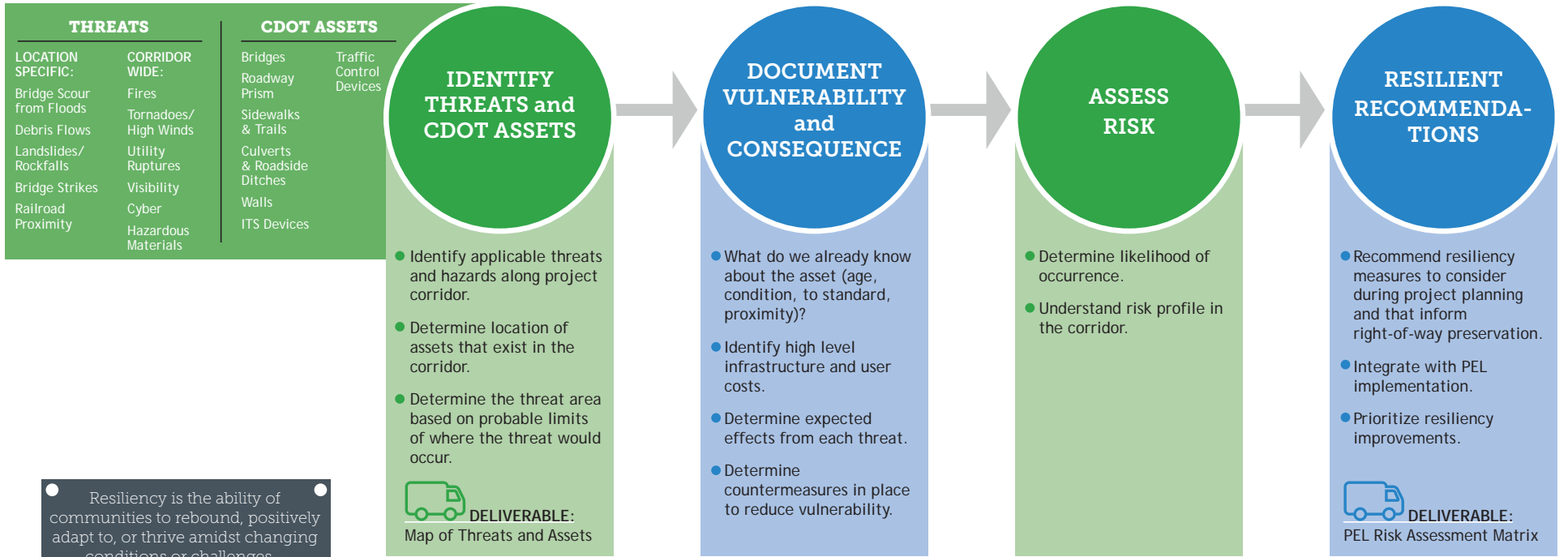
Mid-Term 5-15 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	5A	Section-wide	Monitor and/or perform a warrant analysis at intersections where signals are identified as allowed in the ACP (WCR 11, WCR 11.5 when constructed)
	5A	WCR 11 to WCR 13	Improve the highway from 2 to 4 lanes, add a median, and install turn lanes where warranted at intersections.

Beyond Horizon Year +20 years	LOCATION ON SH 66 (intersection or section)		RECOMMENDED IMPROVEMENT
	5	Section-wide	Increase capacity of highway from 2 to 4 lanes.
	5	WCR 13	Grade-separate intersection.



Identify future resiliency opportunities.

PEL STUDY



Resiliency is the ability of communities to rebound, positively adapt to, or thrive amidst changing conditions or challenges - including disaster and climate change - and maintain quality of life, healthy growth, durable systems, and conservation of resources for present and future generations.

- Colorado Resiliency Working Group -

Once funds have been identified, resiliency opportunities during the PEL process will be used to consider implementing improvements.

PROJECT DELIVERY

